

Appendix E: Facility Equity Analyses

March 3, 2025: Operations and Maintenance Facility Project Update and Engagement Plan (see Attachment C: Fixed Route Maintenance and Charging Facility Title VI Equity Analysis)

January 5, 2026: Receive Public Comments; Adopt Findings; Adopt a Final Initial Study/Mitigated Negative Declaration, Mitigation Measures, and a Mitigation Monitoring and Reporting Program for the Zero Emissions Bus Operations and Maintenance Facility Project; and Approve the Project (see Title VI summary on page 2 and references on pages 5, 6, & Attachment A, page 6)



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March 3, 2025

Honorable Board of Directors
Marin County Transit District
3501 Civic Center Drive
San Rafael, CA 94903

Subject: Operations and Maintenance Facility Project Update and Engagement Plan

Dear Board Members:

Recommendation

Receive an update on the Marin Transit Electric Bus Charging and Maintenance Facility and the Draft Facility Engagement and Communication Scoping Plan.

Summary

Marin Transit has a long-standing need for a centralized bus charging and maintenance facility. Marin Transit acquired property at 1075 East Francisco Boulevard in San Rafael in November 2024. Since then, staff has made steady progress in several aspects of development of our Electric Bus Charging and Maintenance Facility (Facility). Staff has initiated the environmental process; initiated scoping and started to develop partnerships to deliver the workforce and childcare elements of our grant; and developed a Draft Facility Engagement and Communication Scoping Plan (Engagement Plan) per Board action taken in October 2024. In March, staff will provide your Board with updates on project development progress and the Engagement Plan.

Background

Marin Transit's biggest need is for a centralized fixed route facility for maintenance, parking, and charging of zero-emission buses. This facility will support continued investment in electric buses, improve operational efficiency, increase competition for contracted service, and reduce long term risk. Marin Transit received a \$31.5 million Federal Transit Administration (FTA) grant in 2023 that provides funding towards the project and an investment in workforce development. In November 2024, Marin Transit finalized the purchase of a 3.5-acre site at 1075 East Francisco Boulevard in San Rafael (Property).

On October 24, 2024, your Board approved the Property purchase along with a commitment to include a robust community engagement and communication process in development of the Project, in partnership and collaboration with the City of San Rafael and other community organizations. The action stated that Marin Transit would develop a plan for this process no later than March 31, 2025. The October 2024 Board action is included as *Attachment A*.



Discussion

Project Development Update

Marin Transit is in the environmental project phase and is using a task order under the District’s general engineering contract to complete work related to California Environmental Quality Act (CEQA) and National Environmental Policy Act (NEPA) requirements. The consultant team is completing technical studies to understand the project’s potential impacts on traffic, air quality, noise, and cultural resources. In accordance with NEPA requirements, staff has documented the site selection process that the Board participated in over the last 10 years, completed a Title VI analysis, and mailed a postcard to neighbors within a ¼ mile of the property. The Title VI analysis, included as *Attachment C*, found no disparate impact.

To help develop the project’s required programming elements, staff have toured recently built facilities of other public transit agencies. Based on the preliminary space requirements for these elements, staff anticipates the space constraints of the site will limit the number of transit vehicles that can be parked and what ancillary uses can be considered. A high-level timeline for the project is shown below. Pending the completion of the Environmental phase and the security of the federal funding for additional work, staff anticipates releasing a formal solicitation for design work in late 2025. This timeline will allow the District to open the facility by 2030 and provide the charging infrastructure needed to meet the District’s Zero Emission Fleet Transition Plan.



Workforce Development and Childcare

The FTA grant includes \$2.1 million in funding to be spent on workforce development and childcare to support transit operations and the community. Staff has made progress on refining the scope and options for implementing these grant components. Staff met with the Marin Child Care Council.¹ The Marin Child Care Council has a variety of valuable resources that Marin Transit can draw on to develop a robust childcare strategy and they provided suggestions for two programs that would increase availability of childcare and provide direct support to transit workers. Staff also met with Community Action Marin and Canal Alliance, both of whom have excellent resources the team can draw on to fulfil the workforce and childcare aspects of the grant. The Workforce Alliance of the North Bay is another resource that can assist with a variety of programs that could benefit Marin Transit beyond the anticipated grant funded effort. Staff will continue to work with these partners to develop the workforce and childcare components of the plan and update the Board as these plans develop.

Draft Facility Engagement and Communication Scoping Plan

Staff has developed a Draft Facility Engagement and Communication Scoping Plan (*Attachment B*) pursuant to the Board action in October. This Plan was developed in close collaboration with the City of San Rafael and community partners including Canal Alliance and Community Action Marin. The name

¹ Marin Childcare Council website states: “Marin Child Care Council's Mission is to improve the availability, accessibility, and affordability of quality early childhood education and care”; referenced 2/6/25: <https://www.mc3web.org/>.



of the Plan was selected for two reasons. First, the word “engagement” indicates two-way channels of communication and a true commitment to both educate and receive feedback, whereas “outreach” can imply simply one-directional flow of information. Second, this document is called a scoping plan because it is a framework for Marin Transit’s commitment to robust community engagement and communication. This framework will feed into a scope to hire a team to assist in delivering engagement and communication activities over the lifetime of the project’s development. A more detailed Plan will be developed by this team that includes all the specifics of how/when/where activities will be delivered; this way the full team responsible for delivering the activities will be engaged in crafting the Plan for those activities.

The contracted support team could include consultants as well as other organizations/individuals. Beyond traditional consulting firms, Marin Transit anticipates contracting with individuals and/or organizations that have experience with innovations in reaching hard-to-reach populations and with techniques that yield effective, authentic, meaningful engagement by historically marginalized communities.

The Draft Facility Engagement and Communication Scoping Plan outlines **goals** for the engagement and communication process, **techniques and activities** that the team will utilize, and key **audiences** for these activities. These three elements provide a framework for scoping and hiring a team to support Marin Transit staff in delivering engagement activities.

Marin Transit has many other projects and upcoming changes that need rider and community engagement, such as the transition to Next Generation Clipper, potential service changes associated with the MASCOTS effort, Marin Access program changes, and more. Staff plans to maximize the utility of the efforts and contracted support resources outlined in this plan to get feedback on multiple topics from the community, to avoid outreach fatigue and ensure that the community is educated and able to provide meaningful input on topics of most interest to them.

Coordination with Partners

Marin Transit staff coordinated closely with partner agency staff in the development of the Facility Engagement and Communication Scoping Plan. City of San Rafael staff, Canal Alliance, and Community Action Marin had the opportunity to review the plan and discuss in multiple meetings with District staff. The attached plan incorporates the majority of their comments.

The main exception is that the plan does not include engaging the community in a broader land use conversation that was requested by City of San Rafael staff. Marin Transit staff does not recommend broadening the outreach process beyond what Marin Transit can commit to delivering on the site to avoid creating community expectations that cannot be met. Marin Transit is committed to building our facility within the financial resources available and to meet CARB requirements for a zero-emission fleet. Marin Transit staff has and will continue to review technical constraints and opportunities to evaluate feasibility of ancillary and compatible uses on the constrained site. The plan includes the language from the Board action which committed to the “exploration of potential for housing, retail, community electric chargers, or other supportive transportation uses in conjunction with the needs of the District’s transit use of the site.”

Marin Transit staff will continue to coordinate with our community partners to implement effective community engagement across the spectrum of issues facing the District over the next two years. Staff plans to build on the successful engagement efforts our partners have undertaken in the Canal



neighborhood, leveraging consulting resources and engagement processes recently or currently in use, to ensure that community members can meaningfully participate in the District's decision-making.

Fiscal/Staffing Impact

No Fiscal Impact.

Respectfully Submitted,

A handwritten signature in black ink, appearing to read "Cathleen".

Cathleen Sullivan
Director of Planning

Attachment A: Board Action October 24, 2024

Attachment B: Draft Facility Engagement and Communication Scoping Plan

Attachment C: Fixed Route Maintenance and Charging Facility Title VI Equity Analysis

Attachment D: Operations and Maintenance Facility Project Update and Engagement Plan Presentation

Amended action approved by the Marin Transit Board on October 24, 2024

Adopt the findings specified in this report and exercise the agency's authority under state law to overrule the City's disapproval with Marin Transit's commitment to include a robust community engagement and communication process, in partnership and collaboration with the City of San Rafael and other community organizations, the plan for which would be determined no later than March 31, 2025. The community engagement process would include, among other things, exploration of:

- The potential for housing, retail, or other supportive transportation uses in conjunction with the needs of the District's transit use of the site as one or more of the alternatives studied
- The potential for the facility to serve as a Community Electric Resilience site in the event of an emergency or disaster
- The potential for high quality design elements supporting the gateway to the Canal neighborhood
- The potential for circulation, streetscape and pedestrian/bike improvements to the site and in support of the Bellam Blvd. improvements currently being studied by TAM
- The potential for adding or relocating the existing bus stop on Bellam Blvd. with rider feedback to improve traffic and pedestrian flow near the potential future roundabouts on Bellam Blvd.
- The potential for pedestrian circulation and safety improvements on the site including sidewalk widening and lighting on E. Francisco Blvd and the surrounding area, and explore a potential new pedestrian connection from E. Francisco to Kerner Blvd along the southern perimeter of the property
- The potential for enhancements and expansion of existing childcare facilities and workforce development programs in San Rafael that would serve transit workers and community needs



Draft Facility Engagement and Communication Scoping Plan

Updated - 2/26/2025

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Background

Marin Transit has a fleet of 79 buses and serves 3.1 million passengers each year with 15 bus routes, paratransit services, and innovative community-based transportation programs. Marin Transit is subject to a state mandate to electrify our fleet by 2040 and has a long-standing need for a maintenance and operations facility for our buses (“Project”). Marin Transit has received a federal grant to build an electric bus operations and maintenance facility and has purchased a 3.5-acre property at 1075 E. Francisco in San Rafael (“Property”); the grant also includes funds for workforce development and childcare. Marin Transit desires to maximize the benefits and utility of this Project for our agency and maximize the benefits of the Project and our public transit services for the community.

Board Action on Community Engagement

On October 24, 2024, the Marin Transit Board took action related to the Property that included a commitment to include a robust community engagement and communication process in development of the Project, in partnership and collaboration with the City of San Rafael and other community organizations. The action stated that Marin Transit would develop a plan (“Plan”) for this process no later than March 31, 2025. The action stated that the community engagement process would include, among other things, exploration of:

- The potential for housing, retail, or other supportive transportation uses in conjunction with the needs of the District's transit use of the site as one or more of the alternatives studied
- The potential for the facility to serve as a Community Electric Resilience site in the event of an emergency or disaster
- The potential for high quality design elements supporting the gateway to the Canal neighborhood
- The potential for circulation, streetscape and pedestrian/bike improvements to the site and in support of the Bellam Blvd. improvements currently being studied by TAM
- The potential for adding or relocating the existing bus stop on Bellam Blvd. with rider feedback to improve traffic and pedestrian flow near the potential future roundabouts on Bellam Blvd.
- The potential for pedestrian circulation and safety improvements on the site including sidewalk widening and lighting on E. Franciso Blvd and the surrounding area, and explore a potential new pedestrian connection from E. Francisco to Kerner Blvd along the southern perimeter of the property
- The potential for enhancements and expansion of existing childcare facilities and workforce development programs in San Rafael that would serve transit workers and community needs

Facility Requirements

To ensure a successful project for Marin Transit, there are certain project elements that are fundamental to ensure the Project meets the needs of the agency's fleet, including, but not limited to: bus maintenance bays, bus washing, bus charging, parts storage, driver breakrooms and facilities, and contractor administrative office space. Marin Transit originally sought a larger site to accommodate our operations and maintenance facility. After a decade-long search where over 60 properties were explored, this is the Property that was available and met Marin Transit's minimum criteria for the facility.

Based on a bus maintenance facility planning tool provided by the American Public Transportation Association (APTA) and Marin Transit staff tours of several peer agency sites, comparable agencies have significantly more space to work with than Marin Transit has on this Property. Due to the smaller size of the Property, it will require design creativity to fit all the minimum required components on the Property and the allocation of space will be very constrained. In the design process, Marin Transit must prioritize fitting the required facility components on the site before any other site uses are considered. There will be opportunities for community engagement on design and complementary components such as art, placemaking, and circulation improvements around the site as detailed in the Goals section below.

Introduction

This document is the beginning of Marin Transit's commitment to include a robust community engagement and communication process throughout the development of the Project. This document outlines some of the key components of the Plan, including:

- Goals for Process
- Key Audiences
- Draft Techniques and Activities

This document provides the framework for engagement activities. It will feed into a more detailed plan that includes all the specifics of how/when/where the activities will be delivered and how many of each type of activity. This plan will be developed in collaboration with a contracted support team that could include consultants as well as other organizations/individuals. Beyond traditional consulting firms, Marin Transit anticipates contracting with individuals and/or organizations that have experience with innovations in reaching hard-to-reach populations and with techniques that yield effective, authentic, meaningful engagement by historically marginalized communities. These include organizations/individuals such as Canal's Policy & Civic Engagement (PACE) team, Voces del Canal, and/or the graduates of recent capacity building efforts led by Transportation Authority of Marin.

The development of the Project will span multiple years and have several phases. The community engagement process is going to be iterative, and the Plan will be a living document that will evolve over the lifetime of this project. This document is intended to meet the Board deadline and serve as a starting point for discussions with the City of San Rafael and other community organizations to partner and collaborate on engagement and communication processes. We anticipate continuing to closely work with these partners as we contract with a broader team of engagement support services and develop a more detailed plan.

Draft Goals of Facility Engagement and Communication Process

The primary objective of robust community engagement and communication is to complete the best possible project for Marin Transit and the community as a whole that all sides see as a success.

Below are Marin Transit's draft goals for community engagement and communication.

- **Build General Project Awareness** - Build awareness about the need for this project, the benefits of Zero Emission Buses and our public transit services, as well as project elements are critical to success and project constraints, e.g.:
 - Critical Project Components and Constraints – Elements of the operations and maintenance facility that are critical to the development of a useful and functional operations and maintenance facility, including space allocation needs, on-site circulation needs, and utility of project elements. The Property is smaller than was originally desired by Marin Transit. It is feasible to fit everything that is needed to meet Marin Transit's

- maintenance needs on this site, but to meet the functional needs of the operations and maintenance facility for our fleet on a small site will require maximizing the use of space for required elements of the project.
- Mandates to electrify our fleet and how critical this facility is to meet the timeline in our fleet transition plan.
 - Potential for Other Uses: Evaluate and share results of potential for co-location of other site uses (housing, retail, community electric chargers etc.).
- **Evaluate and Solicit Input on Project Impacts and Benefits** – Provide information on how the project may benefit and/or impact the neighborhood. Request input and feedback, receive and respond to any community concerns. Anticipated benefits and impacts include the following (additional benefits and impacts may be identified during the community engagement process):
 - Traffic circulation – How the project improves/degrades/changes traffic and vehicle circulation in the project area.
 - Pedestrian and bicycle circulation and network integration – How the project can improve and not degrade pedestrian and bicycle circulation, e.g. explore benefits and feasibility of pedestrian connector from E. Francisco to Kerner Blvd.
 - Neighborhood integration and identity – Identify opportunities and solicit ideas for incorporating art and other aesthetic and functional elements into conceptual and final designs that reflect the community and cultural context. This includes elements such as lighting, building scale and character and aesthetics, e.g. incorporate high quality design elements that help this site serve as a gateway to the Canal neighborhood. Share project renderings, e.g. building renderings, fencing renderings, etc. to get input and feedback from the community and respond to that feedback.
 - Noise – How the project will change noise levels in the area, if any.
 - **Gather Feedback on Options for Grant Fund Expenditures** – Provide the community with a clear understanding of the scope and scale of funding available through the grant, including any key requirements or constraints. Solicit input to inform decisions on how to best allocate the grant funds in alignment with Marin Transit and community needs and priorities.
 - Solicit input on complementary project components, including workforce development and childcare.
 - Include exploration of potential for housing, retail, community electric chargers, or other supportive transportation uses in conjunction with the needs of the District’s transit use of the site.

Key Audiences for Engagement and Communication

Some of the key audiences for this project are disadvantaged members of our community, and people often disenfranchised from public engagement and government processes. The project team will make targeted efforts to solicit input from these voices, e.g. members of the Canal neighborhood, and seek to incorporate feedback from these voices to the greatest extent feasible. Marin Transit will endeavor to create a trusted feedback loop so that the community knows what input we receive and how that impacts decisions.

- Canal neighborhood residents
- Marin Transit riders
- Project neighbors and businesses in immediate vicinity of project
- Local community organizations
- General public
- Marin Transit Board Members
- The City of San Rafael

Draft Engagement and Communication Techniques

The following are techniques the project team plans to use and the audiences for which they are intended. These have been selected based on techniques that have been successful in past efforts and will take advantage of existing outreach forums to reduce outreach fatigue. Further, we will take advantage of existing meetings and use these opportunities to solicit input from community members on a variety of Marin Transit topics of interest to the community. Marin Transit will also solicit input on these techniques from community partners and use additional strategies if deemed desirable and feasible.

To ensure robust participation, Marin Transit will offer incentives that remove barriers to engagement and demonstrate our commitment to valuing residents' time and input, e.g. transit passes and meals, stipends, and childcare at events/meetings where appropriate and feasible. In person engagement will be designed to reach community members who have varying work schedules and levels of digital access; meetings will be scheduled at times that will accommodate varying work schedules, such as hosting in person outreach events in the evening and on weekends. Marin Transit will learn from past experiences (ours and those of peers) and consult with community nonprofit partners and the public to identify effective locations and times.

The team will seek to provide effective and accessible information to allow the community to provide informed and educated input into the Project. The team will work with community partners to leverage the civic engagement capacity already in place in the Canal neighborhood to assist in developing and implementing an effective program. Overall, Marin Transit seeks to create a two-way/multi-way channel of communication with the audiences identified above to both

educate/inform and receive input/feedback to maximize Project benefits and the success of the Project. All outreach and engagement on this project will be bilingual. The team will be audience-sensitive with development of information, e.g. using images and other accessible techniques to convey complex information to lay/non-English speaking audience. All communications, workshops and other activities will be designed to reach and be accessible to both Spanish and English speakers.

- **Project Website and Electronic Communication (e.g. social media, e-blast):** The project website is an opportunity to display project information and solicit input. Links to the website can be sent via email and social media, and/or included on postcards or other physical educational collateral. The project website will include project goals, project description, project timeline, engagement/communication plan, ways to provide input and contact the project team, including upcoming events/meetings, and links to relevant project documents such as a project factsheet. Marin Transit will develop an email listserv to distribute key project updates and allow anyone to sign up for this list.

Target Audiences: All

- **Community Meetings:** Community meetings will be used to solicit input directly from people via in person presentations, displays and conversations. The project team will endeavor to hold these in accessible, convenient locations and convenient times/days for the intended audiences. Meetings will be used to both educate and receive feedback. Marin Transit will also consider targeted focus groups with specific populations if appropriate to gather detailed input on key topics such as childcare and workforce development.

Target Audiences: Canal neighborhood residents, general public, project neighbors, local community organizations.

- **Participation in Existing Community Events and Meetings (farmers markets, etc.):** The team will participate in existing community events to reach people “where they are at” and reach a broad section of the public. This may also include intercept surveys or pop-up engagement efforts at bus stops or high traffic locations, such as San Rafael Transit Center, grocery stores, schools, food banks, and/or Farmer’s Markets. These events will be used to build general project awareness and to get feedback on project elements. Marin Transit will consult with community nonprofit partners to identify effective locations and times.

Target Audiences: Canal neighborhood residents, general public, project neighbors, local community organizations.

- **Community Partner Meetings and Collaborations:** Marin Transit will meet with key stakeholders on an ongoing basis to educate them on the project and solicit general input, as well as feedback on specific project elements. Marin Transit will also contract with local nonprofits and community-based organizations to facilitate or co-facilitate focus groups and surveys. This approach will leverage trusted relationships and ensure cultural competency.

Target Audiences: City of San Rafael (staff and elected officials), community organizations.

- **Marin Transit Board Meetings:** Board meetings are open to the public, broadcast via Zoom and recorded. These meetings will be used to provide project updates to the Marin Transit Board and general public. Formal projects actions are taken by Board members at these meetings.

Target Audiences: Marin Transit Board, general public.

- **Participation in Related Project Planning:** The project team will participate in and coordinate with other local planning such as the Bellam Blvd. Circulation Project, the Canal Alliance Neighborhood Resilience Project, Nuestro Canal Nuestro Futuro (a community-driven initiative to create a unified vision for the Canal that reflects residents' strengths, needs, and shared dreams), and other planning/engagement efforts.

Target Audiences: General Public, Canal neighborhood.

- **Notifications:** Marin Transit will notify neighbors in the immediate vicinity of the project at key project milestones.

Target Audiences: project neighbors

- **Follow-up on How Feedback is/is not Incorporated:** The project team will summarize feedback received, highlight which ideas are being incorporated, and explain decisions transparently to build trust and accountability and illustrate how public input is shaping the project.

Target Audiences: all.

2025 Engagement and Communication Activities

The following are current and upcoming project engagement and communication efforts in 2025.

- Project Website online by January: www.marintransit.org/facility
- February Postcard mailing to the neighbors
- March Marin Transit Board presentation on the project
- May Nuestro Canal, Nuestro Futuro tabling event
- Late Spring/Summer 2025 Community Meeting
- Summer 2025 Tabling at summer events, e.g. San Rafael 4th Street Farmers market, Pickleweed summer events
- Participation in TAM's Bellam Blvd Circulation Project
- Community Partner Meetings
- Workforce Development and Childcare Working Group



Fixed Route Maintenance and Charging Facility Title VI Equity Analysis

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Background

The Marin County Transit District (Marin Transit or District) provides the local transit service in Marin County, immediately north of the Golden Gate Bridge in Northern California. Marin Transit has a fleet of 70 buses used for fixed route service and serves 3.0 million passengers each year with 19 bus routes. The District also provides paratransit services and other innovative community-based transportation programs to meet the needs of those who cannot use the fixed route bus network.

Marin Transit owns all of its vehicles, but relies on three purchased contract operators to operate fixed route and demand response public transit services. Until recently, Marin Transit did not own any facilities, which drove the need for many small contracts with requirements to provide associated transit vehicle parking and maintenance facilities. The District has made three small property purchases, but still needs a large site for parking, charging and maintenance for the District's 30- to 40-foot-long transit buses.

To meet climate goals and California Air Resources Board (CARB) requirements, Marin Transit is converting from diesel buses to electric under its Zero Emission Bus (ZEB) Plan. As part of the ZEB Plan, the District has acquired six electric buses and associated charging infrastructure. While charging stations and solar power are provided at the smaller sites owned by the District, a larger site is needed for full electrification of the District's fleet.

Over the past 9+ years, Marin Transit has considered and evaluated 67 properties. In 2023, Marin Transit received a federal grant to build an electrified operations and maintenance facility ("grant"). Following the grant award, Marin Transit identified the top three sites for evaluation against the Board-adopted site selection criteria. Subsequently, a willing seller of a well-located 3.5-acre property at 1075 E. Francisco in San Rafael approached the District. Marin Transit was able to purchase the property at 1075 E. Francisco. This equity analysis considers siting of the District's fixed route facility at these four sites.

History of Property Search

In June 2015, the Marin Transit Board of Directors authorized District staff to begin formally negotiating terms to purchase a site for an operations and maintenance facility. Since then, the District has considered and evaluated 67 properties. The site evaluation criteria, approved by the Board in October 2016, served as a guiding document in every evaluation, determining which sites were worth pursuing further. Nine appraisals were conducted during this time, 15 offers were made, and three properties were secured. The 3 secured properties are all smaller sites that are useful to the District, but do not meet the need to have a main site for the maintenance and charging of the District's electric fixed route fleet.

In 2017, the District performed a cost benefit analysis on the potential operations and maintenance facility. The analysis determined that the facility would save \$7 million in operations and maintenance costs over the first 10 years of being in operation. Furthermore, it concluded that the facility would improve on-time preventative maintenance, service reliability, and service delivery, as the cost savings would lead to more funding availability for revenue service.

On October 1, 2019, the California Air Resources Board Innovative Clean Transit (ICT) regulation took effect, which will require 100% of the District's new bus purchases to be zero-emission starting in 2029. This created a deadline for completion of the operations and maintenance facility, which will be necessary to provide charging infrastructure to the large number of electric buses. District staff estimate that facility design and construction needs to begin in 2025 to meet this deadline.

In 2023, the District received the federal grant to fund site acquisition and construction of the operations and maintenance facility. With the ICT deadline rapidly approaching, District staff used the Board-approved evaluation criteria to focus on three preferred sites. Although the District holds the power of eminent domain, site acquisition through voluntary sale was strongly preferred. When the owner of 1075 E. Francisco approached the District about voluntary sale of the site, District staff performed a preliminary evaluation, determined that the site scored comparably to the preferred sites on the evaluation criteria, and pursued the sale.

Title VI Requirements

The Federal Transit Administration (FTA) requires a Title VI Equity Analysis prior to the siting of maintenance facilities and operations centers. The analysis must examine whether the facility siting would result in a disparate impact on the basis of race, color, or national origin; use census tract or block group data as appropriate; and it must compare alternatives. If a disparate impact is found, the analysis must provide substantial legitimate justification for the project siting, and the least discriminatory alternative must be chosen. Outreach must also be performed to persons potentially impacted by the facility siting.

The District's [Title VI program](#) establishes a [disparate impact threshold](#) of 20%, meaning that agency actions "are determined to have a disparate impact on minority populations if, cumulatively, the benefits of the changes accrue to minority populations 20% less than to non-minority populations, or the adverse effects of the changes accrue to minority populations 20% more than to non-minority populations."

Adverse Effects

The adverse effects of a bus operations and maintenance facility are typically considered to be noise associated with maintenance activities and air pollution associated with bus circulation and idling on the site. All four of the selected sites are in industrial neighborhoods where there are no residential parcels close enough to the site to be affected by noise concerns. Furthermore, the proposed facility would ultimately be 100% electric, so there would be no air pollution long term. However, there would be a transition period of about 10 years where the facility would serve a combination of electric and biodiesel hybrid vehicles.

For more information about the proposed facility, see the [Bus Maintenance & Storage Facility Fact Sheet](#).

Selected Sites

The four sites selected for further analysis are:

- Alternative 1: 131 Shoreline Pkwy, San Rafael, CA 94901
- Alternative 2: 8121 Binford Rd, Novato, CA 94945
- Alternative 3: 1151 Andersen Dr, San Rafael, CA 94901
- Alternative 4: 1075 Francisco Blvd E, San Rafael, CA 94901

Baseline Scenario

To determine whether each selected site would create a disparate impact as defined by the Title VI requirements, each alternative must be compared to a baseline scenario. The proposed maintenance and charging facility would replace 3 sites currently used by the District's fixed route operators for parking and maintenance:

- 1011 Andersen Dr, San Rafael, CA 94901
- 1301 Andersen Dr, San Rafael, CA 94901
- 8 Lovell Ave, San Rafael, CA 94901

These sites will be considered together as the baseline scenario.

Disparate Impact Analysis

The disparate impact analysis considers the concentration of minority residents around each selected site (Alternatives 1-4) and compares them to the baseline scenario. The concentration of minority residents is considered at the block group level. Block groups are the smallest geographical unit for which the US Census Bureau publishes demographic data; they are designed to encompass a population size between 600 and 3,000 residents. Because of the target block group population, they can still be quite geographically large in sparsely population industrial areas. For an alternative to have a disparate impact, the concentration of minority residents must be greater than the disparate impact threshold (20%) higher than the baseline concentration. Table 1 shows the results of this analysis.

Table 1: Disparate Impact Analysis

Alternative	Block Group(s)	Total Residents	Minority Residents	Percent Minority Residents	Disparate Impact?
Baseline Scenario	060411122022 060411121001	1,305	1,086	83%	N/A
Alternative 1 (Shoreline Pkwy)	060411122022	593	572	96%	No
Alternative 2 (Binford Rd)	060411011001	2,459	691	28%	No
Alternative 3 (Andersen Dr)	060411122022	593	572	96%	No
Alternative 4 (Francisco Blvd E)	060411122022	593	572	96%	No

Source: American Community Survey 2023, Table B03002, 5-year estimates. Minority residents are defined as residents not considered to be Non-Hispanic White.

As shown in Table 1, none of the selected alternatives would create a disparate impact on the basis of race, color, or national origin.

Public Outreach

The following outreach was done as part of the site selection process to gain feedback from the public, to discuss options with Community partners and to get targeted feedback from neighbors of the preferred site. Since the site selection process has been ongoing since 2016, Marin Transit staff has also met with City of Novato, been the subject of newspaper articles, and spoken with property owners not listed below.

Outreach Events

Marin Transit has participated in community events to educate the public about our services and our need for a facility. Examples of events in the last year are:

- October 19, 2024, Nuestro Canal Nuestro Futuro Launch Event
- September 12, 2024, TAM Clean Fleet Expo

Community Partner Meetings

Marin Transit has met with key stakeholders to educate them on the project and solicit general input, as well as feedback on specific project elements. Examples of meetings are listed below.

Coordination Meetings with City of San Rafael

- August 29, 2023, Facility Siting Meeting with San Rafael; Need, Benefits, Site Evaluations
- October 30, 2023, Need, Impact on City, Sites Evaluated

- February 15, 2024, Meeting with Micah Hinkle, San Rafael Economic Development Department, Site Evaluation, Project Need, and Project Benefits
- December 13, 2024, Coordination Meeting to discuss Engagement Plan
- January 27, 2025, Coordination Meeting to discuss Engagement Plan
- February 13, 2025, Coordination Meeting to discuss Engagement Plan

Meetings/Engagement with Other Partners

- March-April 2023, Pre-Grant Meetings and Surveys with Contract operators
- August 29, 2024, Canal Alliance & Community Action Marin
- February 19, 2025, Canal Alliance & Community Action Marin
- Discussions and collaborations with partners on grant support: Canal Alliance and contractors, Sustainable San Rafael, Cool the Earth

Marin Transit Board Meetings

Board meetings are open to the public, broadcast via Zoom and recorded. These meetings were used to provide project updates to the Marin Transit Board and to the general public. Meeting Dates and Purpose:

- October 17, 2016, Goals and Criteria for Siting a Marin Transit Bus Maintenance and Operations Facility, Approve the goals and criteria for evaluating sites (Action Item)
- August 21, 2017, Cost Benefit Evaluation for Marin Transit Operations and Maintenance Facility (Accept Report)
- December 7, 2020, Bus Maintenance and Storage Facility Needs, Authorize General Manager to negotiate (Action Item)
- January 1, 2021, Facility Needs (Information Only)
- January 10, 2022, Bus Maintenance and Storage Facility Needs, Authorize General Manager to make non-binding offers (Action Item)
- May 2, 2022, Purchase Site for Future Bus Parking and Charging (3010 Kerner) (Action Item)
- May 1, 2023, Zero Emission Bus Rollout Plan Adoption (Action Item)
- August 5, 2024, Approve 1075 E. Francisco Property Purchase (Action Item)
- October 7, 2024, Site Conformity Resolution (Action Item)
- March 3, 2025, Update on Project and Engagement Plan

Notifications

Marin Transit sent post cards to neighbors within a ¼ mile of the 1075 Francisco Project site to notify them of the purchase of the property and receipt of the grant and opportunities to stay engaged and make comment.

Mailing Date: February 14, 2025

Conclusion

None of the selected sites for the Marin County Transit District's proposed fixed route maintenance and charging facility would create a disparate impact on the basis of race, color, or national origin. The District has conducted public outreach as part of the project including the examples listed above.



Zero-Emission Operations and Maintenance Facility

PROJECT UPDATE AND ENGAGEMENT PLAN

Agenda

Information Item

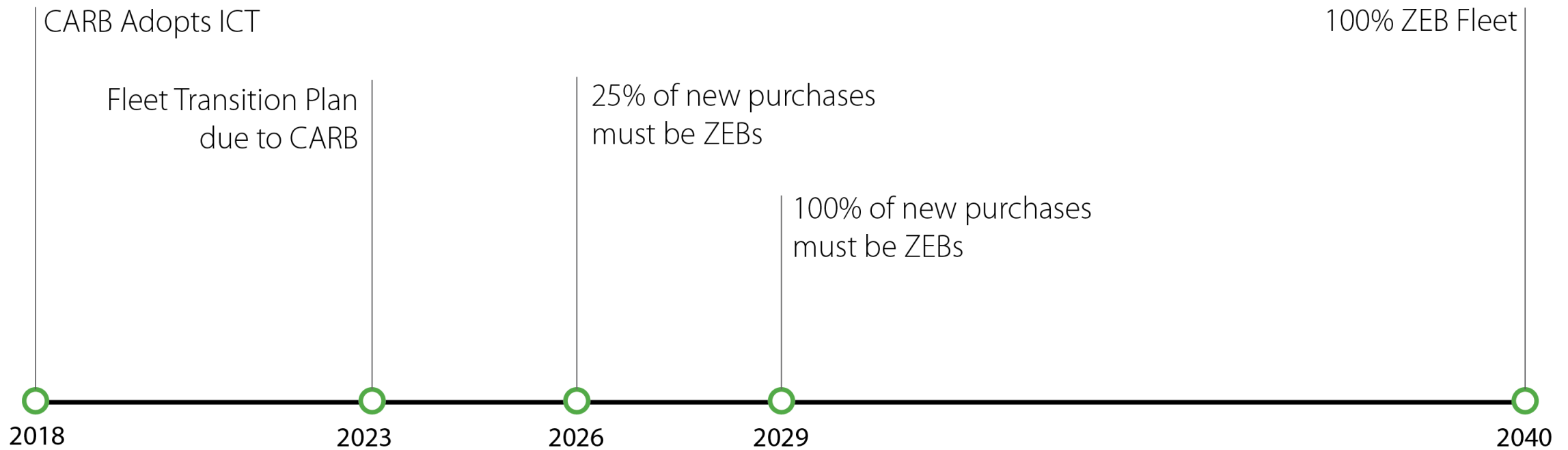
- 01** Background
- 02** Project Update
- 03** Engagement and Communication Plan
- 04** Next Steps

Background

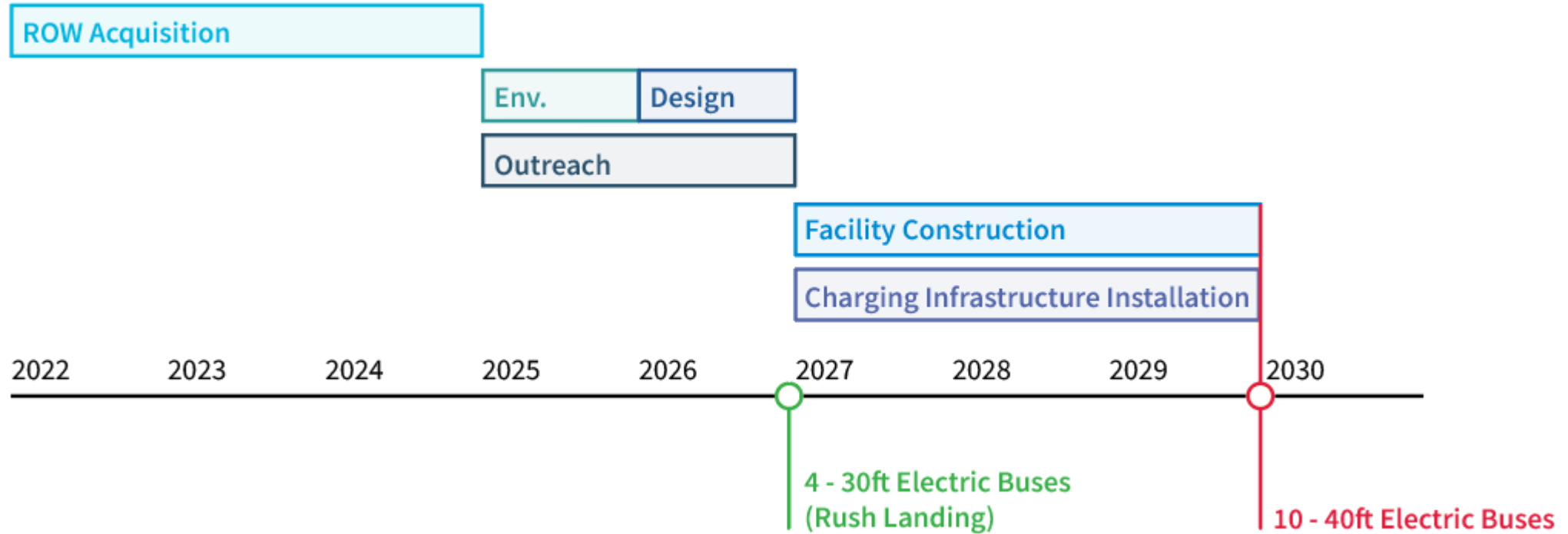
- Long-standing need for facility: Property search started nearly a decade ago
- 2023 FTA grant: \$31.5 M including \$2.1 M for workforce/childcare
- Nov 2024: Purchased 3.5-acre property at 1075 E. Francisco Blvd.
- Oct 2024: Last Board update, action included commitment to develop outreach plan by March 31, 2025 in collaboration with partners

Background: CARB's Innovative Clean Transit Regulation

- 100% Zero Emission Transit Bus Fleets by 2040
- Mandates purchasing percentages for agencies



Background: Fleet Transition Plan



Background: Facility Needs

- Bus maintenance bays (ideally 6)
- Bus washing
- Bus charging (ideally spaces for 50 buses)
- Parts storage
- Driver breakrooms and facilities
- Contractor administrative office space
- Employee parking

**Must stay on schedule, within budget,
and within 3.5-acre space constraint**



Project Update

- Currently in Environmental project phase to fulfil California Environmental Quality Act (CEQA) and National Environmental Policy Act (NEPA) requirements.
- Completed Title VI analysis, found no disparate impact.
- Mailed a postcard to neighbors in a ¼ mile of the property as initial notification.
- Made progress on partnerships and options to approach workforce and childcare components of grant.



Draft Facility Engagement and Communication Plan Development Process

- Developed in collaboration with City of San Rafael and community partners including Canal Alliance and Community Action Marin
- Incorporated lessons learned and best practices from past and current activities
- Utilize existing forums and trusted pathways to honor community time and needs
- Incorporated majority of suggestions/comments from partners
- One area of disagreement with San Rafael about alternatives uses of site:
 - Marin Transit: Exploration grounded in what is possible within constraints (timeline, funding, site), build trust by being clear about what is realistic; responds to language in Board action
 - San Rafael: Desires broader, unconstrained community conversation about range of uses for site not limited by known constraints

Draft Facility Engagement and Communication Plan

- Includes: Goals, Techniques/Activities, Audiences
- Establishes framework for scoping and hiring a team to support Marin Transit staff in delivering engagement activities
 - Exploring partnerships with individuals/organizations with proven experience reaching historically marginalized communities
 - Use techniques that yield effective, authentic, meaningful engagement with riders and community
- Utilize time and resources to maximize benefit for community: seek input on other critical issues that impact neighborhood, e.g. fare transitions, Next Generation Clipper, MASCOTS service changes, bus stops, other issues of interest to participants.

Examples of More Detailed Plan

**Round 1 of
Community Outreach**

- 1 community meeting
- 5 rounds of tabling/intercept surveying
- 1 round of community postering and canvassing

**Round 2 of
Community Outreach**

Targeted outreach focused on specific topics determined from Feasibility Analysis and Decision Space Matrix



**Focus Groups/
Community Partner
Meetings Focused On**

- Public Art
- Workforce Development and Childcare Working Group
- Mobility and Transportation

Next Steps

- Spring 2025: Onboard an outreach support team
- Late Spring 2025: Develop detailed plan
- Summer 2025: Launch outreach activities
- Late 2025: Release formal solicitation for design work
 - pending completion of Environmental phase

Thank you

CONTACT

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January 5, 2026

Honorable Board of Directors
Marin County Transit District
3501 Civic Center Drive
San Rafael, CA 94903

Subject: Receive Public Comments; Adopt Findings; Adopt a Final Initial Study/Mitigated Negative Declaration, Mitigation Measures, and a Mitigation Monitoring and Reporting Program for the Zero Emissions Bus Operations and Maintenance Facility Project; and Approve the Project

Dear Board Members:

Recommendation

Staff recommends that the Board receive public comments; adopt Findings (described below); adopt a Final Initial Study/Mitigated Negative Declaration, including its Response to Comments, mitigation measures, and a Mitigation Monitoring and Reporting Program for the Zero Emissions Bus Operations and Maintenance Facility Project (see [Attachment A](#)); and approve the Project.

Background

To meet climate goals and California Air Resources Board (CARB) requirements, Marin Transit is converting from diesel buses to electric under its Zero Emission Fleet Transition Plan. As part of the plan, Marin Transit has already acquired six electric buses and installed the associated charging infrastructure. The deadline to meet the CARB requirements is June 2030.

Marin Transit's biggest need is for a centralized fixed route facility for maintenance, parking, and charging of zero-emission buses. This facility will support continued investment in electric buses, improve operational efficiency, increase competition for contracted service, and reduce long term risk.

Marin Transit has been engaged in the process to identify the necessary facilities for over 10 years. Marin Transit considered and evaluated more than 67 properties as potential alternative locations to meet its facility's needs.

In 2023, Marin Transit received a \$31.5 million Federal Transit Administration (FTA) grant to acquire property and build an electrified operations and maintenance facility. The deadline to obligate the funds is June 2026.



A willing seller approached Marin Transit about the purchase-sale of a 3.5-acre site at 1075 East Francisco Boulevard in San Rafael. In October 2024, the Marin Transit Board approved the purchase of property. In November 2024, Marin Transit closed on the purchase of the property.

The site is currently raw land. Prior to Marin Transit's purchase, the then-owner planned to construct a hotel on the property. In 2020, the City of San Rafael granted entitlements for a 185-room hotel development (with 195 passenger vehicle parking) for a portion of the site and a vehicle storage lot for the remainder of the site. The City concluded that the proposed hotel development would not result in any significant environmental impacts (including traffic, noise, air quality, or public utilities and services) and required no mitigation measures. Prior to the planned hotel use, the site was used for storage of up to 400 vehicles for nearby auto dealerships under a Temporary Use Permit issued by the City of San Rafael. Before that, in 2006, the City entitled the site for a car dealership (sales and services facility), which was never constructed. Before the car dealership entitlement, the site was developed with a different auto dealership, which was demolished in 2005. Thus, for nearly 20 years before Marin Transit's acquisition, the site was used for vehicle storage and related purposes or sat vacant.

As required by state law, in August 2024, Marin Transit requested the City issue a report on the proposed property acquisition's conformance with the City's General Plan. In September 2024, the City issued its responsive report, concluding that Marin Transit's acquisition of the property and potential future use for an electric vehicle bus charging and maintenance facility would not be consistent with the City's General Plan – Community Commercial Mixed Use land designation. In October 2024, the Marin Transit Board considered the City's report and found that the property acquisition and proposed project is: (1) consistent with the San Rafael General Plan climate change and mobility elements, and equity goals; (2) harmonious with surrounding land uses; and (3) critical to meeting the statewide ZEV bus fleet transition requirements and continuing to provide transit service in Marin County at existing service levels. Accordingly, the Board exercised the agency's authority under state law to overrule the City's disapproval.

In March 2025, Marin Transit completed a Fixed Route Maintenance and Charging Facility Title VI Equity Analysis and presented the report to your Board. The analysis examined whether the facility siting would result in a disparate impact on the basis of race, color, or national origin; using census tract or block group data as appropriate; and compared alternatives. The Title VI analysis found no disparate impact.

In October 2024, along with the Marin Transit Board's approval of the property purchase, the Board approved a commitment to include a robust community engagement and communication process in development of the Project, in partnership and collaboration with the City of San Rafael and other community organizations. In March 2025, the Marin Transit Board considered the Draft Facility Engagement and Communication Scoping Plan, which was developed in close collaboration with the City of San Rafael and community partners including Canal Alliance and Community Action Marin. That same month, the City and Marin Transit executed a collaboration agreement. More detailed discussion of outreach and engagement is provided below.



Project:

Project Title	Zero Emissions Bus Operations and Maintenance Facility Project
Lead Agency/Project Sponsor/ Project Owner	Marin County Transit District
Project Location	1075 East Francisco, San Rafael, CA 94901
Property APN	009-191-37 and 009-191-38
General Plan Designation	Community Commercial Mixed Use
Zoning	General Commercial

Marin Transit proposes to construct a Zero Emissions Bus Operations and Maintenance Facility (Project) on a 3.5-acre site located at 1075 Francisco Boulevard East in the City of San Rafael (Project Site). Marin Transit acquired the undeveloped site from Marin Hospitality in a voluntary sale in November 2024. The Project Site was historically used as a car dealership dating back to the 1960s and has been unused for around 10 years. Marin Transit proposes to construct a zero emissions bus facility to maintain a vehicle fleet of 68 fixed route buses, install bus charging infrastructure, and park approximately two thirds of the total fleet. These services support the fixed route fleet providing local transit service to primarily low-income and minority riders. Currently, Marin Transit operates and maintains this fleet from several different yards and facilities, creating inefficiencies and added costs.

Environmental Review:

A final Initial Study and Mitigation Negative Declaration (IS/MND) has been prepared for the project (see [Attachment A](#)). The IS/MND identifies mitigation measures to reduce the potential environmental impacts of the project to less than significant levels. The mitigation measures would be adopted in a Mitigation Monitoring and Reporting Program (MMRP) and made conditions of approval of the project (see [Attachment A](#)). The areas in which mitigation measures have been identified are:

- Air Quality
- Cultural Resources
- Geology, Soils, and Seismicity
- Noise
- Tribal Cultural Resources

Detailed discussion of the potential environmental impacts and mitigation measures is provided below.

The Notice of Intent (NOI) to adopt a CEQA Initial Study/Mitigated Negative Declaration was placed in the newspaper on November 3rd, 2025. In addition, the NOI was posted with the State Clearinghouse (No, 2025110114), mailed to 18 responsible agencies, posted at Marin Transit's offices, posted on Marin



Transit’s website, posted on the project site, sent to neighbors and interested parties. Written comments were invited between November 3, 2025 through 5:00 pm on December 2, 2025 (Public Review Period).

Two comment letters (both from the City of San Rafael) were received during the Public Review Period; one comment letter (from Canal Alliance) was received after the Public Review Period. No other comment letters have been received as of the date of production of this Board letter (December 30, 2025). In addition, Marin Transit provided the public an opportunity to comment on the IS/MND at the Board meeting on December 1, 2025. No public comments were provided at that meeting. Responses to all three comment letters have been prepared (see Attachment A).

Public Outreach, Engagement, and Noticing, and Community Coordination:

Marin Transit has performed significant public outreach, engagement, and noticing as part of its development of this Project. Coordination with the City of San Rafael and community based organizations has been important for developing effective community outreach and for understanding the local expertise and programs available for workforce development and childcare (elements of Marin Transit’s grant). The following table is a brief summary of just some of the public outreach and noticing the agency has completed:

Date/Timeframe	Action/Event/Item
October 2016	Marin Transit Board Meeting - Goals and Criteria for Siting a Marin Transit Bus Maintenance and Operations Facility, Approve the goals and criteria for evaluating sites - noticed public meeting
August 2017	Marin Transit Board Meeting - Cost Benefit Evaluation for Marin Transit Operations and Maintenance Facility - noticed public meeting
December 2020	Marin Transit Board Meeting - Bus Maintenance and Storage Facility Needs, Authorize General Manager to negotiate - noticed public meeting
January 2021	Marin Transit Board Meeting - facility needs - noticed public meeting
January 2022	Marin Transit Board Meeting - Bus Maintenance and Storage Facility Needs, Authorize General Manager to make non-binding offers - noticed public meeting
May 2023	Marin Transit Board Meeting - Zero Emission Bus Rollout Plan Adoption - noticed public meeting



August 2023	City of San Rafael - meetings re facility siting meeting; need, benefits, site evaluations
October 2023	City of San Rafael - meetings re need, impact on City, sites evaluated
February 2024	City of San Rafael - meetings re site evaluation, project need, project benefits
July 2024	City of San Rafael, Canal Alliance, and Community Action Marin - meetings re site, potential use, and equity issues
August 2024	Canal Alliance and Community Action Marin - meetings re project overview, workforce development, and childcare elements
August 2024	City of San Rafael - letter re proposed action on site purchase - published on City website
August 2024	Marin Transit Board Meeting - approve purchase of property at 1075 E Francisco - noticed public meeting
September 2024	TAM Clean Fleet Expo - educate the public about services and need for a facility
October 2024	City of San Rafael City Council Meeting - General Plan conformity report - noticed public meeting
October 2024	Nuestro Canal Nuestro Futuro Launch Event - educate the public about services and need for a facility
October 2024	Marin Transit Board Meeting - consideration of and action on City of San Rafael General Plan conformity report - noticed public meeting
December 2024 – February 2025	City of San Rafael - meetings re coordination/discussion of engagement framework
January 2025	Launch of project-specific website
February 2025	Canal Alliance and Community Action Marin - meetings re engagement and workforce development
February 2025	Title VI Equity Analysis - mailed a postcard to neighbors within a ¼ mile of the property
March 2025	City of San Rafael City Council Meeting - engagement collaboration agreement - noticed public meeting



March 2025	<p>Marin Transit Board Meeting</p> <ul style="list-style-type: none"> - Title VI Equity Analysis - update on Project and engagement plan - noticed public meeting
May 2025	<p>Nuestro Canal Nuestro Futuro event</p> <ul style="list-style-type: none"> - proposed project - potential additional site uses (dot voting opportunity)
May – October 2025	<p>Consultation with interested tribes</p>
July 2025	<p>Marin Transit Board Meeting</p> <ul style="list-style-type: none"> - update on project, programming the site and alternative uses, engagement, draft site plan - noticed public meeting
July 2025	<p>Community Open House in Marin City</p> <ul style="list-style-type: none"> - update on project
July 2025	<p>Community Open House in the Canal Neighborhood</p> <ul style="list-style-type: none"> - update on project - potential additional site uses (dot voting opportunity)
August 2025	<p>On-line survey re community enhancements and additional site uses posted on project website</p>
September 2025	<p>Marin Transit Board Meeting</p> <ul style="list-style-type: none"> - update on project, initial evaluation of additional uses - noticed public meeting
September 2025	<p>Community Conversation</p> <ul style="list-style-type: none"> - virtual meeting to discuss project - potential additional site uses (on-line survey link provided)
September 2025	<p>Marin Mobility Consortium Meeting</p> <ul style="list-style-type: none"> - project update - noticed public meeting
September 2025	<p>Marin Sanitary Service Customer Appreciation Day</p> <ul style="list-style-type: none"> - tabling including information about project
October 2025	<p>Nuestro Canal, Nuestro Futuro Community Fair</p> <ul style="list-style-type: none"> - proposed project, draft site plan
October 2025	<p>Engagement Plan</p> <ul style="list-style-type: none"> - comments from City of San Rafael - final plan submitted to City staff
November 3, 2025	<p>CEQA Notice of Intent published</p> <ul style="list-style-type: none"> - published in newspaper - posted on State Clearinghouse, Marin Transit website - sent to regulatory agencies - sent to neighbors and interested parties



	- General Manager report at Marin Transit Board meeting
December 1, 2025	Marin Transit Board Meeting - opportunity for public comment on CEQA IS/MND - noticed public meeting

Recommended Findings:

Marin Transit staff recommends the Board adopt all of the following findings (collectively, Findings):

Marin Transit proposes to construct the Project on the Project Site.

Pursuant to the provisions of the California Environmental Quality Act of 1970 (Public Resources Code Section 21000 et seq.) (CEQA) and the State CEQA Guidelines (California Code of Regulations, Title 14, Section 15000 et seq.) (CEQA Guidelines), the City prepared an Initial Study and proposed Mitigated Negative Declaration (State Clearinghouse No. 2025110114) (IS/MND) for the Project.

The Project is described in the November 2025 Draft Initial Study/Proposed Mitigated Negative Declaration IS/MND).

On November 3, 2025 Marin Transit issued a Notice of Intent to Adopt a CEQA Initial Study/Mitigated Negative Declaration.

On November 3, 2025 the IS/MND for the Project was distributed to responsible agencies and the public for review and comment for a 30-day period that ended December 2, 2025.

The IS/MND concluded that significant environmental effects on Air Quality, Cultural Resources, Geology, Soils, and Seismicity, Noise, and Tribal Cultural Resources would be avoided or reduced to less than significant levels by mitigation measures (MM) identified in the IS/MND.

Marin Transit received two comment letters during the comment period, and one comment letter following the closed of the public comment period on the IS/MND.

Marin Transit provided the public an opportunity to comment on the IS/MND at Marin Transit's Board meeting on December 1, 2025. No public comments were provided at that meeting.

Marin Transit prepared a Response to Comments memo that clarifies and amplifies the IS/MND; no modifications to the IS/MND were prepared.

On January 5, 2026, prior to taking action on the Project, the Marin Transit Board held a duly noticed public meeting to receive staff's presentation and public testimony, and to consider the information contained in the IS/MND, along with all staff reports, other pertinent documents, and all written and oral statements received prior to and at the meeting.

The Marin Transit Board has independently reviewed and considered the IS/MND together with the comments and the responses to those comments prior to taking action on the Project.



The comments received by the Marin Transit Board do not require major revisions to the IS/MND due to new or substantially more severe significant effects on the environment, and recirculation of the IS/MND is not required.

After careful consideration of the IS/MND, comments on the IS/MND, maps, facts, exhibits, testimony, staff reports, public comments, and other evidence submitted in the matter, the Marin Transit Board:

1. Finds that the IS/MND for the Project has been completed in compliance with CEQA and reflects the independent judgment and analysis of Marin Transit.
2. Finds that, on the basis of the whole record before it, there is no substantial evidence that the Project as proposed and mitigated will have a significant effect on the environment.
3. Finds that the response to comments merely clarify or amplify the IS/MND; no modifications to the IS/MND are required; and, therefore, recirculation of the IS/MND is not required.
4. Adopts the Mitigated Negative Declaration for the Project.
5. Adopts and incorporates into the Project all of the mitigation measures identified in the IS/MND. The Marin Transit Board further makes the mitigation measures, which are listed below, required conditions of approval of the Project to the extent they are within the responsibility and jurisdiction of Marin Transit:
 - a. AIR QUALITY
Mitigation Measure AQ-1: During any construction period ground disturbance, the construction contractor shall implement measures to control dust and exhaust. Implementation of the measures recommended by BAAD and listed below would reduce the air quality impacts associated with grading and new construction to a less than significant level. The contractor shall implement the following best management practices that are required of all projects:
 - All mobile off-road equipment (wheeled or tracked) greater than 50 horsepower used during construction activities shall meet the U.S. EPA Tier 4 final standards. Tier 4 certification can be for the original equipment or equipment that is retrofitted to meet the Tier 4 Final standards.
 - Include construction equipment exhaust controls and measures to control dust and exhaust during construction.
 - All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered two times per day.
 - All haul trucks transporting soil, sand, or other loose material off-site shall be covered.
 - All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited.
 - All vehicle speeds on unpaved roads shall be limited to 15 miles per hour.

- All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
- Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations). Clear signage shall be provided for construction workers at all access points.
- All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.
- A publicly visible sign shall be posted at the project site with the telephone number and person to contact at the City regarding dust complaints. This person shall respond and take corrective action within 48 hours of receiving a complaint. The Air District's phone number shall also be visible to ensure compliance with applicable regulations.

b. CULTURAL RESOURCES

Mitigation Measure CUL-1: Inadvertent discovery of prehistoric or historic resources during construction. If previously unidentified cultural resources are encountered during project implementation, project personnel shall avoid altering the materials and their stratigraphic context. Project personnel shall not collect cultural resources. A qualified professional archaeologist shall be contacted to evaluate the situation. Historic-era resources include stone or adobe foundations or walls; structures and remains with square nails; and refuse deposits or bottle dumps, often located in old wells or privies. Resources associated with Native peoples include, but are not limited to, chert or obsidian flakes, projectile points, mortars, pestles, and dark friable soil containing shell and bone dietary debris, heat-affected rock, and/or human burials. Historic-era resources include stone or adobe foundations or walls; structures and remains with square nails; and refuse deposits or bottle dumps, often located in old wells or privies. If a Native American resource is discovered, the Federated Indians of Graton Rancheria shall be contacted to evaluate the situation in coordination with the qualified professional archaeologist. If the Federated Indians of Graton Rancheria determine that the resource constitutes a tribal cultural resource, they will provide direction for its treatment.

Mitigation Measure CUL-2: Protocol for Human Remains Discovery. In the event that human remains are discovered during excavation and/or grading of the project site, all activity within a 50-foot radius of the find shall be stopped. The County Coroner shall be notified and shall make a determination as to whether the remains are of Native American origin or whether an investigation into the cause of death is required. If the remains are determined to be Native American, the Coroner shall notify the NAHC immediately. Once the NAHC identifies the most likely descendant (Federated Indians of Graton Rancheria), the descendant will make recommendations regarding proper burial, which will be implemented in accordance with Section



15064.5(e) of the CEQA Guidelines. All actions taken under this mitigation measure shall comply with Health and Human Safety Code § 7050.5(b).

c. GEOLOGY, SOILS, AND SEISMICITY

Mitigation Measure GEO-1: Protocol for Paleontological Discoveries. In the event that paleontological resources are unearthed during grading, ground disturbance work shall cease until a qualified paleontologist determines whether the resource requires further study. The qualified paleontologist shall temporarily halt and/or divert grading activity to allow recovery of the resources, and prepare a Paleontological Resources Monitoring Program (PRMP). The area of discovery shall be temporarily contained for evaluation by the qualified paleontologist. Upon completion of the paleontological monitoring program, the qualified paleontologist shall prepare a final monitoring report documenting the results of the monitoring program, which shall include a description of the methods used, fossils collected, and significance of recovered fossils.

d. NOISE

Mitigation Measure NOI-1: Barriers, such as plywood structures or flexible sound control curtains shall be erected along the southern perimeter of the construction site, and around stationary equipment as feasible (i.e., generators, air compressors, etc.) to minimize the amount of noise during construction on Sensitive Receptor No. 1. Perimeter barriers shall be at least eight (8) feet in height and constructed of materials achieving a Transmission Loss (TL) value of at least 15 dB(A), such as ½ inch plywood.

Mitigation Measure NOI-2: The construction contractor shall prohibit the use of heavy construction equipment (such as a large bulldozer or any piece of equipment capable of generating vibration levels of 0.089 PPV and 87 VdB or greater at a distance of 25 feet) to areas at a minimum distance of 140 feet from off-site structure No. 1 (recording studio), or approximately 109 feet from the project site's southern property line. Smaller equipment, such as a small bulldozer, can be used up to the project site's southern property line.

Mitigation Measure NOI-3: If heavy construction equipment (such as a large bulldozer or any piece of equipment capable of generating vibration levels of 0.089 PPV and 87 VdB or greater at a distance of 25 feet) is required to be used within 140 feet of off-site structure No. 1 (recording studio), or approximately 109 feet from the project site's southern property line, the construction contractor shall provide written notice to the recording studio 60 days in advance of such activity. The written notice shall identify the dates of activity, the hours of activity, types of equipment to be used, and the vibration levels anticipated at off-site structure No. 1 (recording studio).

e. TRIBAL CULTURAL RESOURCES

Mitigation Measure TCR-1: Tribal Cultural Monitoring during Ground Disturbing Activities. Tribal monitoring shall occur by the Federated Indians of Graton Rancheria during ground-disturbing construction activities. Ground-disturbing activities include



blading, grading, and trenching, which have a moderate to high potential to expose or impact tribal cultural resources.

The Federated Indians of Graton Rancheria tribal monitor(s) shall observe ground-disturbing activities as described above to look for indications of tribal cultural resources that may be exposed by construction equipment. For safety purposes, the Federated Indians of Graton Rancheria tribal monitor(s) shall generally inspect spoils at a safe distance, in accordance with project health and safety protocols, including the option to enter trenches before they get past five feet deep. The backhoe operator shall shake the spoils from the bucket slowly and spread them out for inspection before adding to the spoils pile. It is the responsibility of the Federated Indians of Graton Rancheria tribal monitor(s) to report tribal cultural resources found within the project boundaries, whether on the surface or subsurface, to the Federated Indians of Graton Rancheria’s Tribal Historic Preservation Officer, who will notify Marin Transit to stop work. Once notified, Marin Transit shall issue a stop-work order in the immediate area of the discovery. Tribal monitor(s) shall not direct construction personnel or equipment. Prior to initiation of ground-disturbing construction activities, a Tribal Cultural Resources Treatment Plan shall be prepared in consultation with the Federated Indians of Graton Rancheria and an archaeologist approved by the Federated Indians of Graton Rancheria to direct monitoring and provide guidance for the treatment of any discoveries.

- 6. Adopts the MMRP for the Project (Attachment A, pages 180-184), and incorporated herein by reference, which incorporates all the mitigation measures identified in the IS/MND.
- 7. Approves the Project.

Next Steps:

If the Board adopts the Findings, adopts the IS/MND, mitigation measures, and MMRP, and approves the Project, staff will proceed with the Project development and delivery process. A high-level timeline for the Project is shown below.



Staff anticipates completing design work in 2026, engaging construction in 2027, and opening the facility and providing the charging infrastructure needed to meet Marin Transit’s Zero Emission Fleet Transition Plan by 2030.



Major upcoming milestones are shown below.

Milestone Dates	Description
December 2025	Release procurement for design team
December 2025	Release Invitation for Bid for surcharge construction
February 2026	Anticipated NEPA approval by FTA
February 2, 2026	Board consideration of award of surcharge construction contract
May 4, 2026	Board consideration of award for facility design contract
Spring 2026	Begin surcharge work to ensure soil consolidation
Spring 2026-2027	Facility design work
June 30, 2026	Deadline to meet September 30, 2026 obligation of remaining \$28.83 million in FTA 5339(b) funds
Summer 2027	End soil consolidation
2027-2029	Facility construction
June 30, 2030	Deadline for facility opening to meet CARB requirements

Approval of the project at today’s meeting will keep the project on schedule and on track to meet 2026 funding deadline and 2030 project delivery deadline.

Respectfully Submitted,

Paul Haifley
Facility Development Project Manager

Attachment A: Final Initiation Study/Mitigated Negative Declaration, including its Response to Comments, and Mitigation Monitoring and Reporting Program, dated December 2025

Attachment B: Fixed Route Facility CEQA Adoption Presentation

Attachment A is 184 pages and thus was not included here. Final IS/MND can be found here:

<https://marintransit.gov/projects/zero-emission-bus-operations-and-maintenance-facility>



Bus Operations and Maintenance Facility

CEQA Adoption of Findings & Project Approval



Topics

- 01** CEQA Notification
- 02** Technical Studies & Key Findings
- 03** Comments Received
- 04** Response To Comments
- 05** Final IS/MND
- 06** Request For Board Adoption & Approval
- 07** Project Timeline Updates

CEQA Notification

Marin IJ

**Postcards
To Neighbors**

**Email
Notification to
Stakeholders**

**Physical
Posting On-
Site**

**Website
Posting**

**Website
Comment
Form**

Technical Studies and Key Findings

Technical Studies	Key Findings	Mitigation Measures
Air Quality	Less Than Significant Impact w/Mitigation	AQ-1: Dust/Exhaust Control Measures
Cultural Resources/ Tribal Resources	Less Than Significant Impact w/Mitigation	CU-1: Inadvertent Discovery of Prehistoric/Historic Cultural Resources CU-2: Protocol for human remains discovery TCR-1: Tribal Cultural Monitoring
Noise and Vibration	Less Than Significant Impact w/Mitigation	NOI-1: Temporary Barriers NOI-2: Prohibition of heavy equipment NOI-3: Written noticing (60 days)
Geology and Soils	Less Than Significant Impact w/Mitigation	GEO-1: Protocol for Paleontological Discoveries
Traffic	Less Than Significant Impact	None

*Notes: All Mitigation Measures to be implemented during construction
TCR-1 implementation during surcharge grading work only*

Comments Received

Three Comment Letters From the Following Agencies:

- City of San Rafael
- Mayor of San Rafael
- Canal Alliance

Comment Subjects by Sender

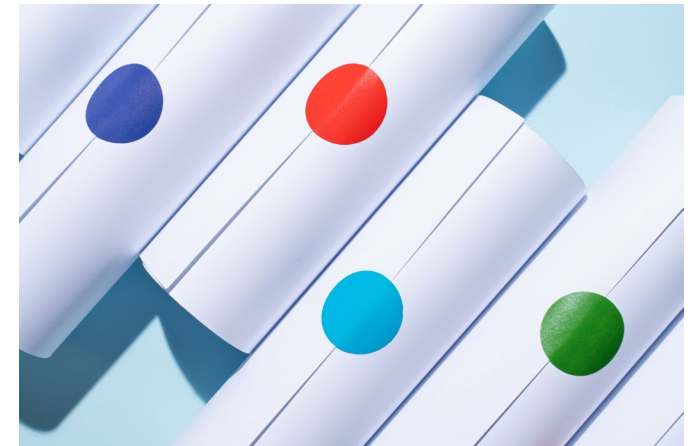
City	Mayor	Canal Alliance
Zoning Analysis	General Plan/Zoning Standards	Sustainable Transportation
Air Quality Impacts		Partnership with Canal Residents
Stormwater Analysis	Vision for Canal Neighborhood	Workforce Development
Traffic Study		
Flood and Sea Level Rise Risk	Surrounding Community Benefits	Clean Transit Solutions
Community Engagement		

Response to Comments

Marin Transit acknowledged all comments and responded in the following ways:

- Provided Clarification
- Identified Location of Requested Information in the Draft IS/MND

Comments did not raise any issues with the adequacy of the IS/MND, and therefore no revisions were necessary.



Final IS/MND

Includes:

- Evaluation of Impacts
- Findings of Less than Significant or No Impacts
- Findings of No Significant Impact with Mitigation Measures Implemented
- Response to Comments
- Final Mitigation Monitoring and Reporting Program (MMRP)

Category	Control/Resource	Mitigation Measures (MM)
Air Quality	Dust/Exhaust Control Measures	AQ-1
	Temporary Barriers	NOI-1
Noise and Vibration	Restrict Heavy Equipment Operations	NOI-2
	Provide 60-Day Advanced Noticing	NOI-3
Tribal Cultural Resources	Surcharge Soil Monitoring & Treatment	TCR-1
	Unanticipated Discoveries	
Cultural Resources	Prehistoric/Historic Resources	CU-1
	Human Remains	CU-2
Geology/Soils	Paleontological Discoveries	GEO-1

Request for Board Approval

On the basis of the whole record before the Board, there is no substantial evidence that the Project as proposed and mitigated will have a significant effect on the environment.

- Receive public comments
- Adopt Initial Study/Mitigated Negative Declaration findings in compliance with CEQA
- Adopt Final Initial Study/Mitigated Negative Declaration, including:
 - Response to Comments
 - All Mitigation Measures
 - Mitigation Monitoring and Reporting Program (MMRP) for the Project
- Approve Project

Timeline



Project Timeline Updates

December 8, 2025	Released Surcharge Construction IFB
December 22, 2025	Released Design RFQ
January 5, 2026	Board considers CEQA/MMRP Adoption
February 2026	Anticipated NEPA Approval
February 2, 2026	Consider Award of Surcharge Construction Contract (Contingent on NEPA Approval)
May 4, 2026	Consider Award of Design contract
Spring 2026	Start Surcharge Construction
Spring 2026-2027	Facility Design Work
June 30, 2026	Deadline to meet September 30, 2026 obligation of remaining \$28.83 million in FTA 5339(b) funds
Summer 2027	End soil consolidation
2027-2029	Facility Construction
June 30, 2030	Deadline for Facility Opening

Thank you

CONTACT

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